

## LONDON BOROUGH OF ENFIELD

### PLANNING COMMITTEE

Date : 16<sup>th</sup> February 2011

**Report of**  
Assistant Director, Planning &  
Environmental Protection

**Contact Officer:**  
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**Ward:** Turkey  
Street

**Application Number :** LBE/10/0031

**Category:** Other Development

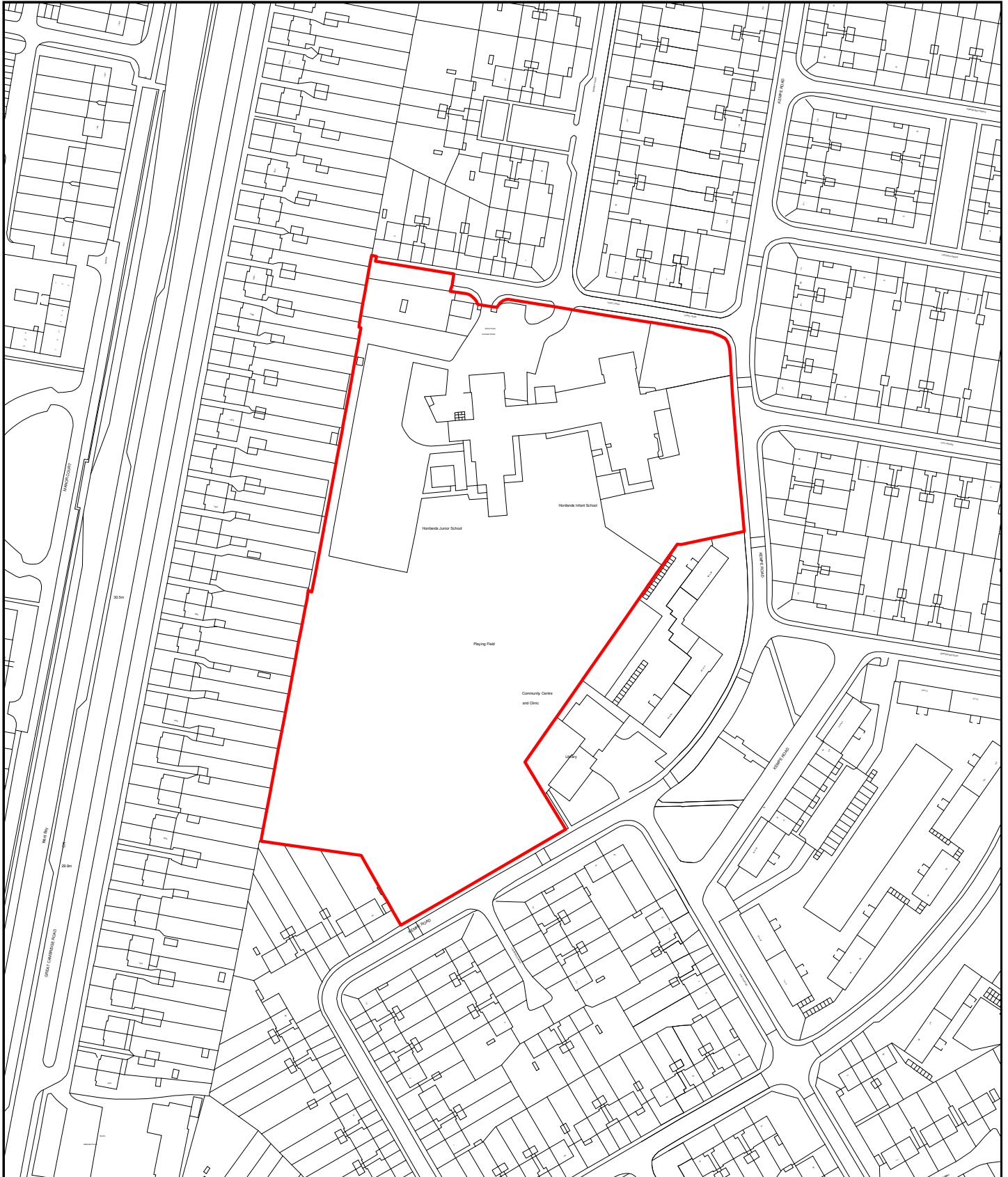
**LOCATION:** HONILANDS PRIMARY SCHOOL, LOVELL ROAD, ENFIELD, EN1 4RE

**PROPOSAL:** Two storey side extension, single storey front extension to provide a new entrance lobby, new car park at side and associated landscape works.

**Applicant Name & Address:**  
London Borough of Enfield  
PO Box 50,  
Civic Centre,  
Silver Street,  
Enfield,  
EN1 3XA

**Agent Name & Address:**  
Ann Lakshmanan,  
Shepherd Epstein Hunter  
Phoenix Yard  
65, Kings Cross Road  
London  
WC1X 9LW

**RECOMMENDATION:** That in light of the objection raised by Sport England to the loss of playing fields and therefore, subject to the views of the Government Office for the West Midlands, planning permission be deemed to be **GRANTED** in accordance with Regulation 3 of the Town & Country Planning General Regulations 1992, subject to conditions:



### Development Control



Scale - 1:2002  
Time of plot: 16:16

Date of plot: 02/02/2011

## **1. Site and Surroundings**

- 1.1 Honilands Primary School is bounded by Lovell Road to the north and Kempe Road to the east and south. To the west are 2-storey semi-detached dwellings fronting the Great Cambridge Road (A10) whilst on Kempe Road to the east and Lovell Road to the north, are small terraced.
- 1.2 The surrounding area is predominantly residential in character, with the rear gardens of the aforementioned semi-detached dwellings and to the southeast by three-storey flats and an associated community hall and library.

## **2. Proposal**

- 2.1 Permission is sought for the erection of a two storey side extension, single storey front extension to provide a new entrance lobby, new car park at side and associated landscape works.

### Two storey extension

- 2.2 The proposed building is made up of three oblongs in a staggered formation, with a 2-storey link between the existing and proposed buildings. The 2-storey extension will provide a total footprint of approximately 513sqm and a floor area of approximately 950sqm. This element of the development will be to a maximum height of approximately 7.5m to the top of curved roofs.
- 2.3 At the gables on both ends of the oblong, the facades are mostly fenestration serving as the main windows of the teaching areas inside. To the northeast, the rooms face the nature area courtyard while to the southwest, they face the playing field. There are generous roof overhangs on the southwest gable ends to provide solar shading and opaque insulated panels within the fenestration.

### Single storey extension

- 2.4 The proposed extension will be sited on the northern elevation of the existing school buildings, and is effectively an infill extension as it will be positioned between the hall and the Head Teacher's office.
- 2.5 It will be between 6.3m and 7m deep, 19m wide, and will have a vaulted roof approximately 4m in height.
- 2.6 This area will accommodate a new entrance lobby, an enlarged office/admin area and a large meeting room.

### Car Park

- 2.7 The proposed new car park will be located in the north-west corner of the school and will cover an area of approximately 480sqm.
- 2.8 It will provide additional parking for an additional 25 vehicles, including 1 disability bay.

### Landscaping

- 2.9 The existing hard play area will extend towards the new extension and south of the existing hutted classroom. The immediate area surrounding the existing mature ash tree will be covered with permeable resin-bound gravel.
- 2.10 A new footpath will extend southwards along the Nursery and Children Centre boundary fence to the entrance gate along Kempe Road.
- 2.11 A structured soft landscape zone is provided along the Nursery and Children Centre boundary fence.

### **3. Relevant Planning Decisions**

- 3.1 An application for a single storey infill extension to provide additional offices and staffroom (TP/05/0804) was granted planning permission on 24<sup>th</sup> June 2005. In addition, a temporary classroom to the east of the site (LBE/09/0017) was granted a limited period permission (expiring on 24<sup>th</sup> June 2014) to provide teaching space whilst building works are completed to the main school building.
- 3.4 Planning permission was granted on 27 July 2010 for the erection of a single storey detached building to south east of site to provide a Nursery with canopy to both sides and front and new pedestrian and vehicle access to Kempe Road and 5 parking bays adjacent to existing Library (ref: LBE/10/0022).

### **4. Consultations**

- 4.1 Statutory and non-statutory consultees
  - 4.1.1 Traffic and Transportation advises that there are no objections but make the following observations:
    - The application is part of an ongoing plan to expand the Borough's schools. The school has been selected for expansion based on its suitability to accommodate an increase in pupils. However, whilst the school may have sufficient space to expand, there could still be a considerable impact on the neighbouring roads in terms of parking provision and traffic generation.
    - Although the school is expanding gradually, the application should be assessed on the ability of the school and surroundings to cope with the total increase in pupils which will peak in Sep 2015.
    - Parking for staff has been addressed by the proposed new car park, which will provide a total of 49 spaces for the total of 95 staff (on completion of the expansion project), which is acceptable and in line with London Plan parking standards, although the travel plan will also be applicable to the staff.
    - In order to address the existing problems and safety concerns then the TA proposed a number of mitigation measures and there is already a 20mph

zone in place. In total six schemes have been proposed, with the three most important ones being identified as:

- Option 6 – CCTC, and Option 2 (eastbound one way arrangement on Lovell Road) – essential
  - Option 1 - Turning head and or increased on street parking on Lovell Road – high priority
  - Option 3 – Parking on Marryat Road, and Option 5, new lay-by on Masons Road – low priority.
- Following discussions with Traffic, it is considered that the schemes that allow for more parking and increase the attractiveness of driving to the school need to be balanced against the objectives of reducing the need to travel by car. Additionally, in terms of road safety around the school, then the eastbound one way arrangement on Lovell Road may not necessarily increase road safety due to the perception that traffic will be unobstructed which may result in increased speeds. Therefore should approval be granted then the measures needed to mitigate against parking problems and road safety will need to be revised.

### Conclusion

- The proposal will result in an increase in both vehicular and pedestrian traffic. The TA accompanying the application predicts the worst case scenario for the school, which is based on the modal split from 2009 continuing throughout the five years of the schools expansion. However, the existing Travel Plan surveys and its subsequent review in 2010 has shown that the modal split is slightly below the percentage used to predict future trips, and lends confidence to the 'soft measures' currently being implemented to reduce car journeys to the school. Hard measures have also been proposed, but the exact package of works required has not been determined yet, although the school should be prepared to fund some mitigation measures should they be necessary.

4.1.2 Environmental Protection & Regulations advise that there are no objections.

4.1.3 Sport England object to the proposed development on grounds relating to the loss of playing field..

4.1.4 Thames Water advise that in relation to sewerage and water infrastructure, there are no objections to the proposal.

### 4.2 Public

4.2.1 Consultation letters have been issued to 92 neighbouring and nearby occupiers. No comments have been received.

## **5. Relevant Policy**

### 5.1 Local Development Framework

At the meeting of the full Council on 10<sup>th</sup> November 2010, the Core Strategy of the Local Development Framework was approved. The document and the policies contained therein, are now material considerations to be taken into

account when considering the acceptability of development proposals. The following are of relevance:

- CP8: Education
- CP9: Supporting community cohesion
- CP20: Sustainable energy use and energy infrastructure
- CP21: Delivering sustainable water supply, drainage and sewerage infrastructure
- CP22: Delivering sustainable waste management
- CP25 Pedestrians and cyclists
- CP26: Public transport
- CP30: Maintaining and improving the quality of the built and open environment
- CP32: Pollution
- CP36: Biodiversity

## 5.2 Saved UDP Policies

After the adoption of the Core Strategy, a number of UDP policies are retained as material considerations pending the emergence of new and updates policies and development standards within the Development Management Document. The following are of relevance:

- (II)GD3 Aesthetics and functional design
- (II)GD6 Traffic
- (II)GD8 Site access and servicing
- (II)H8 Privacy
- (II)H8 Privacy

## 5.3 The London Plan

- Policy 2A.1 Sustainability criteria
- Policy 3A.18 Protection and enhancement of community infrastructure and community facilities
- Policy 3A.24 Education facilities
- Policy 3C.17 Tackling congestion and reducing traffic
- Policy 3C.23 Parking strategy
- Policy 3D.13 Children and young people's play and informal recreation strategies
- Policy 3D.14 Biodiversity and nature conservation
- Policy 4A.1 Tackling climate change
- Policy 4A.2 Mitigating climate change
- Policy 4A.3 Sustainable design and construction
- Policy 4A.6 Decentralised Energy: heating, cooling and power
- Policy 4A.7 Renewable energy
- Policy 4B.1 Design principles for a compact city
- Policy 4B.3 Enhancing the quality of the public realm
- Policy 4B.5 Creating an inclusive environment
- Policy 4B.8 Respect local context and communities

## 5.4 Other Relevant Policy

- PPS1: Sustainable development
- PPG13: Transport
- PPG17: Planning for open space, sport and recreation

## 6. Analysis

### 6.1 Principle

6.1.1 As an existing school, it is considered appropriate for this site in principle, to be utilised to provide additional education development to meet the Boroughs identified need for such accommodation. However, this must be weighed against the presumption of loss of some playing field and also against the potential for increased traffic pressures within the vicinity.

### 6.2 Loss of Playing Field

6.2.1 Paragraph 15 of PPG17, advises that development on playing fields should not be allowed unless:

- i. the proposed development is ancillary to the use of the site as a playing field (e.g. new changing rooms) and does not adversely affect the quantity or quality of pitches and their use;
- ii. the proposed development only affects land which is incapable of forming a playing pitch (or part of one);
- iii. the playing fields that would be lost as a result of the proposed development would be replaced by a playing field or fields of equivalent or better quantity and quality and in a suitable location
- iv. the proposed development is for an outdoor or indoor sports facility of sufficient benefit to the development of sport to outweigh the loss of the playing field

6.2.2 The proposed 2-storey extension will be constructed upon existing playing fields and the objection in principle from Sport England is noted. A justification for the expansion of the school onto the playing fields has therefore been provided and can be summarised as below:

- The school has been identified by the London Borough of Enfield as being suitable for expansion as there is no alternative to expanding Honilands.
- The Education (School Premises) Regulations 1999 sets out the statutory minimum total team playing fields area that should be provided for a school of Honiland's proposed pupil numbers as 20,000sqm.
- Building Bulletin 99: Briefing Framework for Primary School Projects notes that in confined sites, the sports pitch area may be provided on a nearby site and/or through a single all-weather pitch.
- The site has a total site area of 27,508.8sqm. The existing total pitch area available for team sports is 14,628m<sup>2</sup> as broken down in the following table:

	Existing (sqm)	Proposed (sqm)	Building Bulletin 99 (sqm)
Hard play areas	4261	5492	
All weather MUGA	536	536	
Grassed playing field	9831	8940	
Total pitch area	14,628	14,968	12,600-14,925

- The new extension at the school has been designed as a two storey extension to minimise its footprint and impact on the playing field.
- The hard play area has been enlarged to accommodate the increased pupil numbers by incorporating some grassed area that is not part of the playing field area.
- The existing all weather MUGA has been retained
- The proposed grass playing field area is 8,940. There is sufficient space to accommodate a 200m running track during summer and a junior sized football pitch during winter.
- Although the proposals for the extension do reduce the area of playing field, the additional school accommodation cannot be provided in an alternative fashion

6.2.3 The above analysis clearly demonstrates that whilst there will be some loss of existing playing field, this will be compensated for by the increase in size of the 'hard play areas', in accordance with subparagraph iii identified above. It is therefore considered that whilst the objection from Sport England is acknowledged, there is no material harm to on site sports provision and that on balance, taking into account the educational need underpinning this application, the increase in useable playing area compensates for the loss of some open field.

### 6.3 Impact on Character of Surrounding Area

6.3.1 The proposed 2-storey extension is of a good contemporary design, which will provide a focal point when viewed from the playing fields to the south. The use of materials, to be secured via condition, to complement the existing structures will provide a visual link to those existing structures whilst the building will also appear 'detached' due to the 2-storey glazed link. Furthermore, the design of the single storey infill element will also integrate well with the appearance of the existing building.

6.3.2 All elements are considered appropriate to the context of the site and surroundings.

### 6.4 Impact on Neighbouring Properties

6.4.1 The nearest residential units are within the 3-storey block approximately 35m to the south east, with the new nursery/ children's centre in-between. To the west, the nearest element of the proposed two storey extension to the rear boundaries of those properties is approximately 50m, with a further 35m to the dwellings. Distancing is therefore considered to be acceptable.

6.4.2 Due to distancing level detailed above, it is considered that there will be no impact on neighbouring occupiers in terms of loss of light and outlook.

### 6.5 Access and Traffic generation

6.5.1 An additional 180 pupils will generate an increase in both pedestrian and vehicular traffic. In order to predict the impact of the proposed increase in numbers then some form of comparison with baseline conditions is required. The TA in para 3.3 states that baseline studies were informed by a detailed site audit conducted on Tuesday 30<sup>th</sup> June 2009, desktop research carried out



between June and July 2009 and updated in 2010, and through an additional traffic and transport workshop held at the school on Monday 6<sup>th</sup> July 2009.

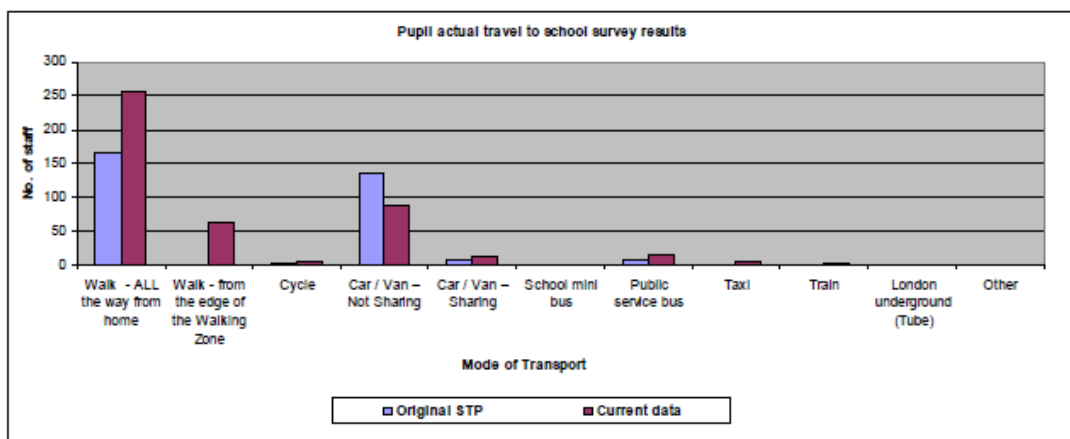
6.5.2 Although the TA details the existing parking restrictions of the site, no data is provided on the actual levels of on street parking during the dropping off and the collection times of the school. The evidence presented in the TA may be accurate but it is mainly anecdotal as opposed to containing quantitative figures on parking levels and available spaces around the school.

6.5.3 The TA does include the modal split and the predicted modal split for the future increase in pupil numbers, using figures taken from the Travel Plan. The data in the Travel Plan is from 2009, although there is more recent data from 2010 contained in the Travel Plan 1<sup>st</sup> Annual Review, which is shown below:

- Pupils: Nursery 57; Reception 87; Year 1 60; Year 2 56; Year 3 60; Year 4 58; Year 5 58; Year 6 60; TOTAL = 496
- Staff: Full time 22; part time 57

Table showing modal split of pupils 2009 and 2010:

Pupil actual mode of transport to school	Original STP	Current data	Modal Split percentage (actual travel) Original STP Data	Modal Split percentage (actual travel) Current Data	Modal Shift + / - %
Walk - ALL the way from home	165	255	52.1	57.6	5.5
Walk - from the edge of the Walking Zone	0	62	0.0	14.0	14.0
Cycle	1	5	0.3	1.1	0.8
Car / Van – Not Sharing	135	87	42.6	19.6	-22.9
Car / Van – Sharing	8	13	2.5	2.9	0.4
School mini bus	0	0	0.0	0.0	0.0
Public service bus	8	14	2.5	3.2	0.6
Taxi	0	5	0.0	1.1	1.1
Train	0	2	0.0	0.5	0.5
London underground (Tube)	0	0	0.0	0.0	0.0
Other	0	0	0.0	0.0	0.0
<b>TOTAL</b>	<b>317</b>	<b>443</b>	<b>100.0</b>	<b>100.0</b>	<b>0.0</b>



Pupil survey Figure 2

6.5.4 The “Original STP” as referred to in the data above is the data from the original surveys done in 2009. The results show that progress, in terms of reducing both the percentage of car journeys to the school and the total number of trips by car, has been positive. Car use has reduced in real terms

from 135 trips to 97 trips, with the 97 trips representing a modal split of 19.6% of all trips. The reduction of the vehicular trips is made up by the increase in walking trips, which increased by a total of 20% (including walking trips from the edge of the walking zone).

- 6.5.5 The figures included in the TA are from 2009, and the modal split for this year is used to predict the future trip patterns. The predicted future trip patterns are shown below (taken from 6.11 of the TA).

**Table 6.11 Future Inbound Pupil Trips**

Time Period	Number of Person Trips by Mode							Total
	Car	Bus	Walk	Rail	Car Share	Cycle / Scooter	Park & Stride	
School Day / Nursery (first session) Start (08:30–08:50)	288	17	351	0	17	0	2	675
Nursery (second session) Start (12:10-12:25)	19	1	24	0	1	0	0	45
<b>Total Daily Arrivals</b>	<b>307</b>	<b>18</b>	<b>375</b>	<b>0</b>	<b>18</b>	<b>0</b>	<b>2</b>	<b>720</b>

*Source: Derived from Honilands CE Primary School STP, January 2009; Note: Arithmetic errors due to rounding.*

- 6.5.7 The total number of pupils is set at 720 as this is the level expected after all the years fill up, including the nursery. The percentage modal split of the above application is based on the existing figures from the 2009 surveys, not the more recent 2010 surveys. The modal split used in the predictions is 42% car, and 52% for walking, when the latest modal split suggests approx 20% arrive by car and 70% walk.
- 6.5.8 As the existing modal split percentage of walking is slightly higher than the split used in the predictions, then there is some degree of confidence that if the Travel Plan is continually reviewed and improved that the resulting number of trips by car after five years will be below those predicted. If the 20% modal split is maintained then the number of car trips would be 122 out of 720, which is fewer than the original surveys done in 2009 for when the school had 317 pupils.
- 6.5.9 There are a number of other factors that could influence the modal split, such as the catchment area for future pupils. The TA suggests in para 5.6 that most pupils live in the EN1 postcode, and that the majority of the intake for September 2009 lived within 2km of the school (although the figures mentioned in 5.3 are not included in the TA). The TA states in para 5.7 that the catchment area for future pupils will be closer to the school than is existing, but this is only based on the September 2009 intake. It is not possible to predict the intake for the future years, and the increase in numbers over the next few years increases the chance of pupils living outside the existing catchment area, and therefore being more likely to arrive by car.
- 6.5.10 The hours of use of the school must also be taken into account when considering the trip generation, as the school operates staggered hours and therefore not all the vehicles will be arriving at the same time. The TA details the operating times of the school which are stated as:

- 8:00am – Pupils arrive for breakfast club;
- 8:30am – Pupils arrive at school;
- 8:50 am – Majority of pupils arrive at school;
- 11:25am - School day ends for nursery (first session);
- 12:25pm - School day starts for nursery (second session);
- 2:55pm - School day ends for nursery (second session);
- 3:00pm - Key Stage 1 (KS1)(Reception and Years 1 & 2) School day ends;
- 3:05pm - Key Stage 2 (KS2)(Reception and Years 3, 4, 5 & 6) School day ends; and
- 4:20pm - Afterschool activities end (approx. 20 pupils)

6.5.11 The nursery has staggered hours, with one session starting 8.50 and finishing at 11.25, and then another session starting at 12.25 and finishing at 2.55. The rest of the school generally finishes between 2.55 and 3.05. It is during the school finishing times that parents are most likely to park and wait to collect their children, which can lead to illegal parking causing problems for local residents. It is also noted that the school is providing a separate entrance in the south east of the site from Kempe Road (in association with the approved nursery / children's centre), which may result in traffic intensity falling as parents will be able to park further away from the existing access.

6.5.12 It is proposed that a suitably worded condition is imposed to secure the mitigation measures as proposed within the Transport Assessment.

## 6.6 Parking

6.6.1 The development will provide for 25 additional parking spaces (inclusive of x1 disability space), leading to the access onto Lovell Road. Parking for staff has been addressed by the proposed new car park, which will provide a total of 49 spaces for the total of 95 staff (on completion of the expansion project), which is acceptable and in line with London Plan parking standards, although the travel plan will also be applicable to the staff.

## 6.7 Sustainable Design and Construction

6.7.1 A BREEAM Pre-Assessment has been submitted, indicating that the scheme is aiming to achieve a minimum rating of 'Very Good'. This is encouraged and a condition is recommended to secure this.

6.7.2 The London Plan stipulates that an Energy Assessment must form part of any major proposal. The assessment should demonstrate expected energy and carbon dioxide emission savings (20%) from energy efficiency and renewable energy measures incorporated into the development (Policy 4A.4).

6.7.3 Whilst the nursery building is not considered a 'major scheme', it is a permanent new building that is part of an expansion programme at the school and therefore should demonstrate a commitment to sustainable design and construction.

6.7.4 A Sustainability Assessment Form has been submitted. Proposed Energy saving measures includes the following:

- Trickle vents;

- The installation of a Heat exchange system;
- Insulation of hot water pipes and tanks;
- Insulation for walls, roof and under-floor to meet with minimum building regulations requirements;
- Time controlled lighting

6.7.5 Whilst the above measures are welcomed, a condition is suggested to establish that the development will achieve energy efficiency in accordance with the above savings target.

## 6.8 Trees

6.8.1 A tree survey has been conducted of the site, with information provided indicating the species, BS categorisation (quality of the tree), those that are to be retained, the root protection area radius, and tree protection.

6.8.2 Seven trees in total are identified for removal as they will be most impacted upon during the development. The loss of these unprotected trees can be mitigated through a condition requiring additional plantings, particularly within the 'ecological zone' to the north-west of the proposed 2-storey extension.

## 6.9 Ecology

6.9.1 An ecological assessment has been submitted as part of the application. It identifies the site as having minimal

6.9.2 Enhancements proposed include the provision of bat / nest boxes, brown roofs, insect boxes positioned on walls, new landscaping, and the provision of standing deadwood / log piles around the pond to create hibernacula for common reptile and amphibian species.

6.9.3 The above measures are welcomed as they not only increase the ecological value of the site but also provide a valuable and visible educational tool. A condition will be imposed to secure ecological enhancements of the site.

## 7. **Conclusion**

7.1 The proposed development will improve facilities at the school in terms of teaching accommodation and in the provision of playing areas. Moreover, the design of the extensions are unique when viewed against the existing structures, with the proposed entrance providing a new focal point from Lovell Road and the 2-storey extension providing a focal point from vantage points to the south of that element. The development should also, through measures proposed, not lead to unacceptable impact on parking and traffic safety within the immediate vicinity of the school. Mindful of these, it is also considered that the educational need and the overall effect on playing field provision having regard to Sport England's objection, is not sufficient to render the scheme unacceptable and thus, the proposed development is considered acceptable and approval is recommended for the following reasons:

1. The proposed development, having regard to the amount of playing field lost to accommodate the extensions and having regard to the increased provision of playing fields, is considered to comply with Core Policies 8 and 34 of the Core Strategy, Policies (II)GD3, (II)CS2 and (II)CS3 of the

Unitary Development Plan, Policies 3A.24, 3D.8 and 3D.13 of The London Plan, with PPS1: Sustainable Development and PPG17: Planning for Open Space, Sport and Recreation.

2. The proposed extensions improve teaching facilities at Honilands Primary School as well as providing for additional teaching space for which there is a recognised shortage within the Borough. It is considered that the proposed development complies with Core Policies 8 and 9 of the Core Strategy, Policies (II)CS1, (II)CS2 and (II)CS3 of the Unitary Development Plan, Policies 3A.18, 3A.24, 3D.13 4B.1, 4B.8 of The London Plan, and with PPS1: Sustainable Development.
3. The proposed extensions having regard to their design, size and siting do not unduly affect the amenities of adjoining or nearby residential properties having regard to Core Policy 30 of the Core Strategy, Policies (II)GD3 and (II)H8 of the Unitary Development Plan and with Policies 4B.1 and 4B.8 of The London Plan.
4. The proposed development should not lead to conditions prejudicial to the free flow and safety of traffic, including pedestrian traffic, on the adjoining highways. In this regard, the proposal is considered to comply with Policies (II)GD6 and (II)GD8 of the Unitary Development Plan and with Policy 3C.23 of The London Plan.

## 8. Recommendation

- 8.1 That in light of the objection raised by Sport England to the loss of playing fields and therefore, subject to the views of the Government Office for the West Midlands, planning permission be deemed to be **GRANTED** in accordance with Regulation 3 of the Town & Country Planning General Regulations 1992, subject to the following conditions:

1. C06 Details of phasing of construction
  2. C07 Details of materials
  3. C09 Details of hard surfacing
  4. C10 Details of levels
- The development shall not commence until details of a landscaping scheme shall be submitted to and approved in writing by the Local Planning Authority. The detailed landscaping scheme shall include the following details:
- a. a revised Access Statement detailing routes through the landscape and the facilities it provides;
  - b. an ecological report detailing how the landscaping scheme maximises the ecological value of the site;
  - c. existing and proposed underground services and their relationship to both hard and soft landscaping;
  - d. proposed trees: their location, species and size;
  - e. soft plantings: including grass and turf areas, shrub and herbaceous areas;
  - f. topographical survey: including earthworks, ground finishes, top soiling with both conserved and imported topsoil(s), levels, drainage and fall in drain types;

- g. enclosures: including types, dimensions and treatments of walls, fences, screen walls, barriers, rails, retaining walls and hedges;
- h. hard landscaping: including ground surfaces, kerbs, edges, ridge and flexible pavings, unit paving, furniture, steps and if applicable synthetic surfaces; and
- i. any other landscaping feature(s) forming part of the scheme.

All landscaping in accordance with the approved scheme shall be completed/planted during the first planting season following practical completion of the development hereby approved. The landscaping and tree planting shall set out a plan for the continued management and maintenance of the site and any planting which dies, becomes severely damaged or diseased within five years of completion of the development shall be replaced with new planting in accordance with the approved details or an approved alternative and to the satisfaction of the Local Planning Authority.

The development shall be carried out strictly in accordance with the details so approved and shall be maintained as such thereafter.

Reason: To minimise the impact of the development on the ecological value of the area, to ensure the development provides the maximum possible provision towards the creation of habitats and valuable areas for biodiversity and to preserve the character and appearance of the area in accordance with Policies CP30 and CP36 of the Core Strategy, the Biodiveristy Action Plan and Policies 3D.14, 4A,3 and 4B.5 of the London Plan 2008.

- 5. C11 Details of enclosure
- 6. C12 Parking and turning facilities shall be provided in accordance with Drawing No.051 rev.2 unless otherwise approved in writing by the Local Planning Authority.

Reason: To ensure the development complies with adopted standards and does not prejudice conditions of safety or traffic flow on the adjoining highway or the internal circulation of the car park.

- 7. C14 Details of access and junction
- 8. C16 Private vehicles only – Parking areas
- 9. C17 Details of landscaping
- 10. C18 Details of tree protection
- 11. C21 Construction servicing area
- 12. C22 Details of construction vehicle wheel cleaning
- 13 NSC1 That development shall not commence until a construction methodology has been submitted to and approved in writing by the Local Planning Authority. The construction methodology shall contain:
  - (i) a photographic condition survey of the roads, footways and verges leading to the site

- (ii) details of construction access, associated traffic management and vehicle routing to the site
- (iii) arrangements for vehicle servicing and turning areas
- (iv) arrangements for the parking of contractors vehicles
- (v) arrangements for wheel cleaning
- (vi) details of the site compound and the layout of temporary construction buildings
- (vii) arrangements for the storage of materials
- (viii) hours of work
- (ix) A construction management plan written in accordance with the 'London Best Practice Guidance: The control of dust and emission from construction and demolition'.

The development shall then be undertaken in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not lead to damage to the existing roads, prejudice highway safety or the free-flow of traffic on the adjoining highways, and to minimise disruption to neighbouring properties.

14. NSC3 Evidence confirming that the development achieves a BREEAM rating of no less than 'Very Good' shall be submitted to and approved in writing by the Local planning Authority. The evidence required shall be provided in the following formats and at the following times:
- a. design stage assessment, conducted by an accredited Assessor and supported by relevant BRE interim certificate, shall be submitted at pre-construction stage prior to the commencement of superstructure works on site; and,
  - b. post construction assessment, conducted by an accredited Assessor and supported by relevant BRE accreditation certificate, shall be submitted following the practical completion of the development and prior to the first occupation.

The development shall be carried out strictly in accordance with the details so approved, shall be maintained as such thereafter and no change there from shall take place without the prior approval of the Local Planning Authority.

Reason: In the interests of addressing climate change and to secure sustainable development in accordance with the strategic objectives of the Council and Policies 4A.1, 4A.2, 4A.3 and 4A.9 of the London Plan as well as PPS1.

15. NSC7 No development shall take place until an assessment has been carried out into the potential for disposing of surface water by means of a sustainable drainage (SUDS) scheme, in accordance with the principles of sustainable drainage systems set out in national planning policy guidance and statements, and the results of that assessment

have been provided to the Local Planning Authority. The assessment shall take into account the design storm period and intensity; methods to delay and control the surface water discharged from the site; and measures to prevent pollution of the receiving groundwater and/or surface waters.

Reason: To ensure that the proposal would not result in an unacceptable risk of flooding from surface water run-off or create an unacceptable risk of flooding elsewhere.

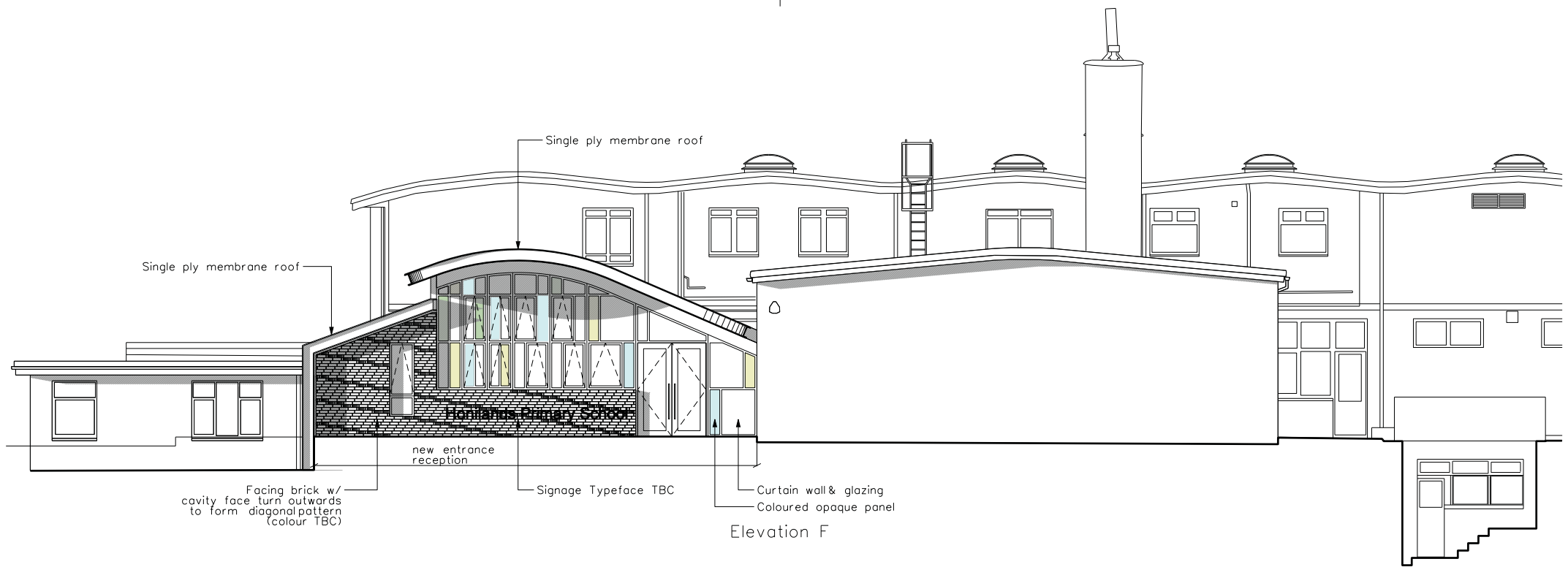
16.NSC8 Surface water drainage works shall be carried out in accordance with details that have been submitted to, and approved in writing by, the local planning authority before the development commences. Those details shall include a programme for implementing the works. Where, in the light of the assessment required by the above condition, the local planning authority concludes that a SUDS scheme should be implemented, details of the works shall specify:

- i) a management and maintenance plan, for the lifetime of the development, which shall include the arrangements for adoption by any public authority or statutory undertaker or any other arrangements to secure the operation of the scheme throughout its lifetime; and
- ii) the responsibilities of each party for implementation of the SUDS scheme, together with a timetable for that implementation.

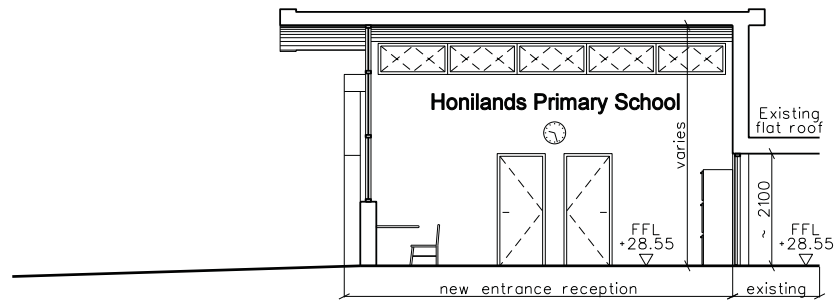
Reason: To ensure implementation and adequate maintenance to ensure that the proposal would not result in an unacceptable risk of flooding from surface water run-off or create an unacceptable risk of flooding elsewhere.

- |          |                              |
|----------|------------------------------|
| 17. C25  | No additional fenestration   |
| 18. C41  | Details of external lighting |
| 19. C51A | Time limited permission      |





Elevation F



Section E



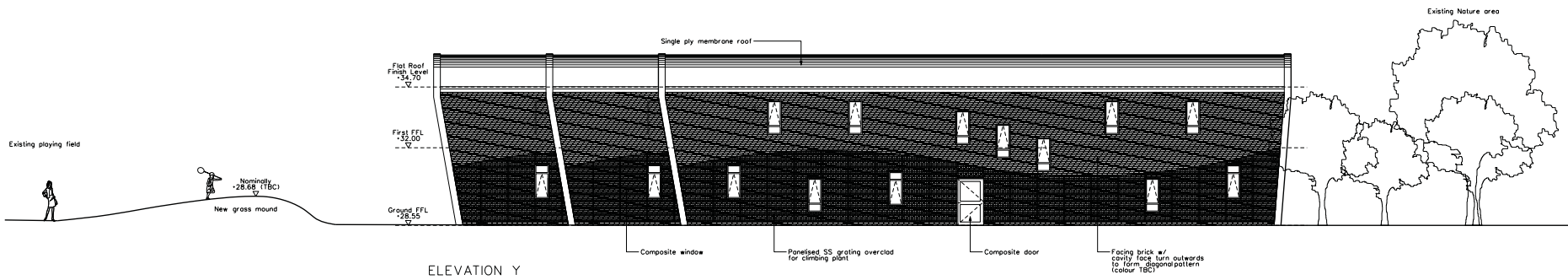
rev	date	description	by
02	24/08/10	Issue for Planning application Aluminium standing seam roof replace with single ply membrane. Roof corner profile amended.	BC
01	25/08/10	General amendment	BC
00	03/08/10	Drawing originated / Initial Issue	BC
drawing amendments			

## Shepherd Epstein Hunter Architecture Planning Landscape

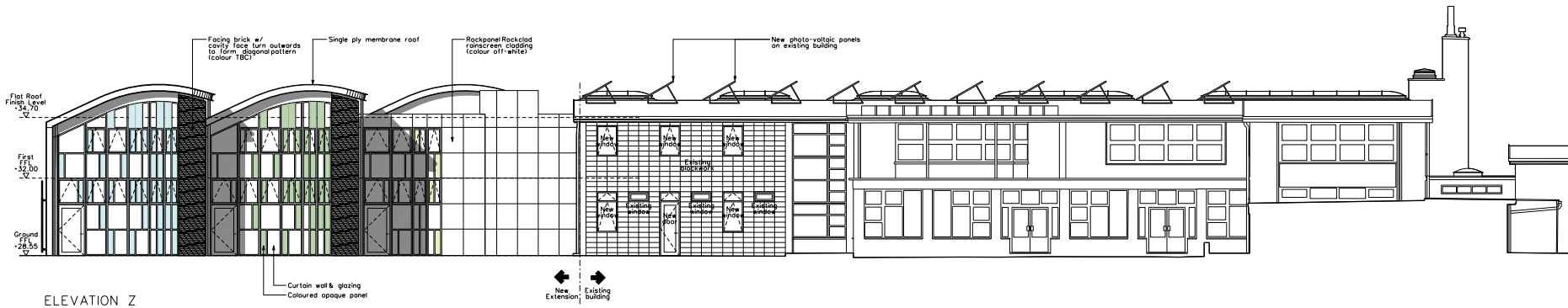
Phoenix Yard, 65 Kings Cross Road, London WC1X 9LW  
tel: 020 7841 7500 fax: 020 7841 7575  
email: architecture@seh.co.uk web: www.seh.co.uk

project	Honilands Primary School
client	London Borough of Enfield

drawing status	Planning	scale	1:100	size	A3
drawing	Proposed Entrance Reception Elevation & Section	job no.	10007	drawing no.	026
		rev.	02		



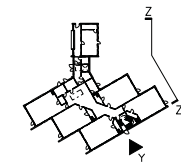
ELEVATION Y



ELEVATION Z



- Notes**
1. Do not scale from this drawing
  2. Work to figured dimensions where shown
  3. Confirm dimensions on site before fabrication or construction - report any discrepancies to CA



00	24/08/10	Issue for Planning application (drawing issued revised). PV panels on existing roof indicated. Aluminium standing seam roof replace with single ply membrane. Roof corner profile amended.	BC
04	18/08/10	Elevation Y mesh overclad changed to grating overclad	BC
03	24/08/10	All glazed facade panels amended & opaque panels added. Flushwork terminals added. Building materials indicated.	BC
02	03/08/10	Table and elevation amended. Roof profile amended.	BC
01	27/06/10	All chimney window created. Plans correct board cladding suggested. Roof valley ridge added.	BC
00	25/05/10	Drawing originated / initial issue	BC
rev	date	description	by

Drawing amendments

**Shepherd Epstein Hunter**  
 Architecture Planning Landscape

Phoenix Yard, 65 Kings Cross Road, London WC1X 9LW  
 tel: 020 7841 7500 fax: 020 7841 7575  
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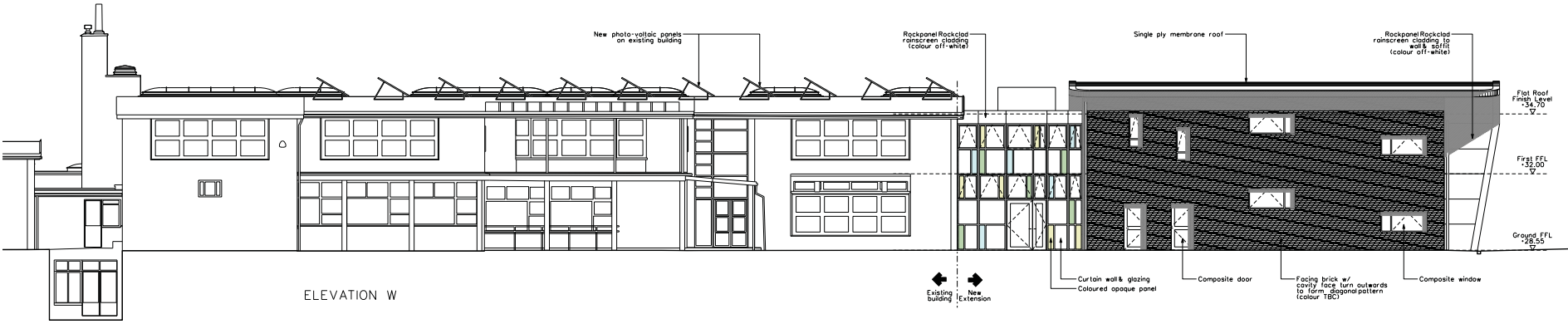
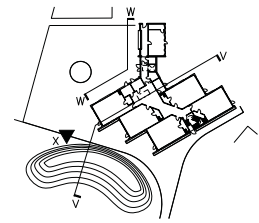
Project  
**Honilands Primary School**

Client  
**London Borough of Enfield**

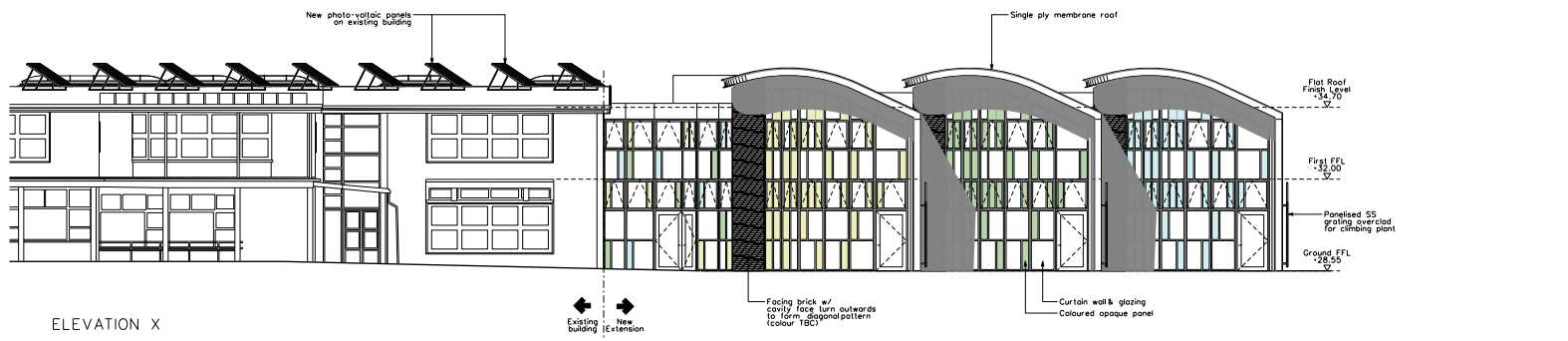
Drawing status	scale	size
<b>Planning</b>	<b>1:100</b>	<b>A1</b>
Drawing	job no.	
<b>Proposed Extension</b>	<b>10007</b>	
Elevation 2 of 2	drawing no.	rev.
	<b>022</b>	<b>05</b>

**Notes**

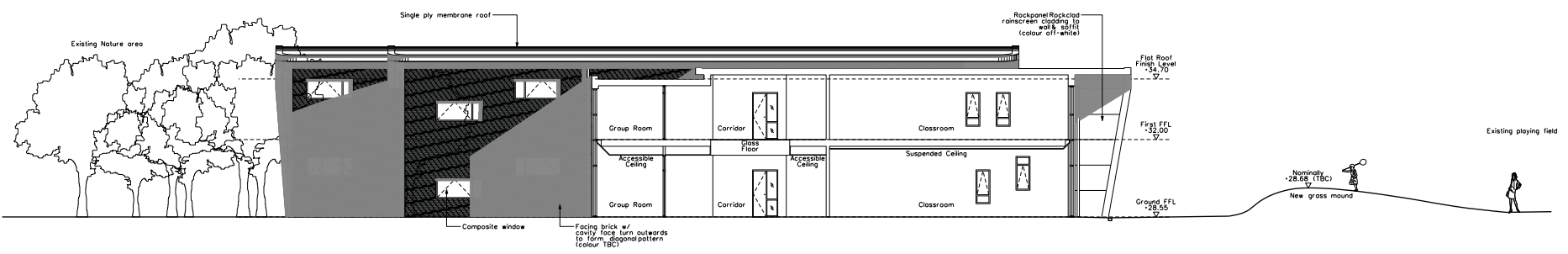
1. Do not scale from this drawing
2. Work to figured dimensions where shown
3. Confirm dimensions on site before fabrication or construction - report any discrepancies to CA



ELEVATION W



ELEVATION X



ELEVATION V



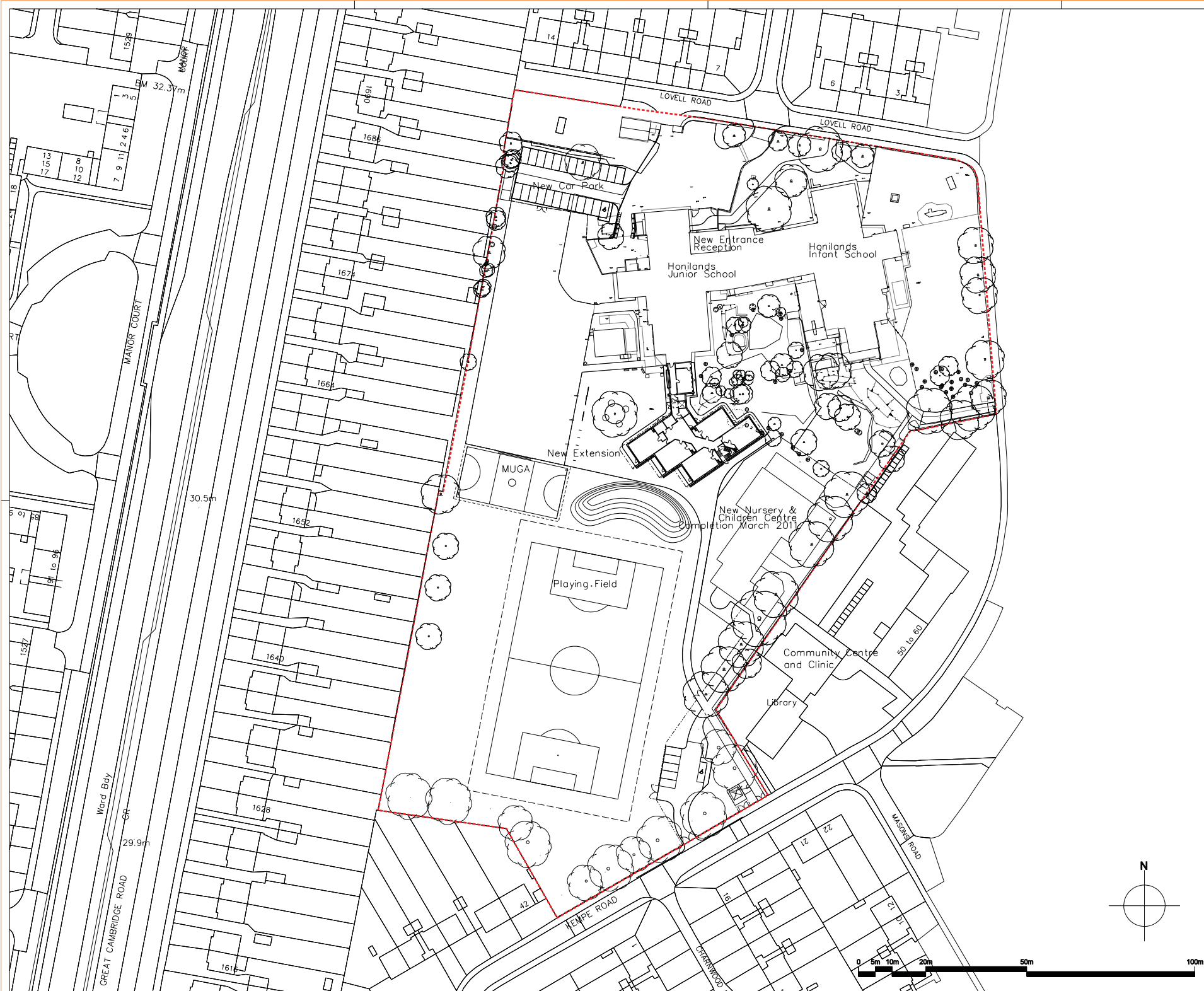
rev	date	description	by
04	24/09/10	Issue for Planning application	BC
03	24/09/10	Drawing layout revised. PVC panels on existing roof indicated. Aluminium cladding panels roof replaced with single ply membrane. Elevation V added. Roof corner profile amended.	BC
02	24/09/10	All glazed window panels amended & opaque panels added. Platform terminals added. Building materials indicated.	BC
01	27/06/10	Table and elevation amended. Roof profile amended. Plans current board cladding suggested. Roof valley ridge added.	BC
00	28/09/10	Drawing originated / initial issue	BC

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Architecture Planning Landscape

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email: architecture@seh.co.uk web: www.seh.co.uk

Project: **Honilands Primary School**  
Client: **London Borough of Enfield**

Drawing status	scale	size
Planning	1:100	A1
Drawing	10007	
Proposed Extension Elevation 1 of 2	021	04



- Notes
1. Do not scale from this drawing
  2. Work to figured dimensions where shown
  3. Confirm dimensions on site before fabrication or construction - report any discrepancies to CA

 Site Boundary

01	24/09/10	Issued for Planning Application	BC
02	28/09/10	Football pitch updated to LRB comments	AL
03	24/08/10	New extension footprint indicated	BC
02	20/09/10	Drawing originated / initial issue	BC
rev	date	description	by
drawing amendments			

**Shepherd Epstein Hunter**  
Architecture Planning Landscape

Phoenix Yard, 65 Kings Cross Road, London WC1X 9LW  
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email: architecture@seh.co.uk web: www.seh.co.uk

project	<b>Honilands Primary School</b>
client	<b>London Borough of Enfield</b>

drawing status	scale	size
<b>Planning</b>	<b>1:500</b>	<b>A1</b>
drawing no.	job no.	
<b>Proposed Site Plan</b>	<b>10007</b>	
drawing no.	rev.	
<b>004</b>	<b>03</b>	